

BYLAW NO. 1194-01
OF THE
TOWN OF BONNYVILLE

"THE TOWN OF BONNYVILLE RAILWAY AREA STRUCTURE PLAN BYLAW"

BEING A BYLAW OF THE TOWN OF BONNYVILLE IN THE PROVINCE OF ALBERTA TO ADOPT AN AREA STRUCTURE PLAN FOR THE TOWN OF BONNYVILLE.

WHEREAS pursuant to Section 633 of the Municipal Government Act; Statutes of Alberta, 1994, Chapter M-26., and any amendments thereto, a Council of a municipality shall pass a bylaw in accordance with the Act.

AND WHEREAS the Council of the Town of Bonnyville deems it proper and expedient to pass such a bylaw.

NOW THEREFORE, COUNCIL OF THE TOWN OF BONNYVILLE, DULY ASSEMBLED HEREBY ENACTS AS FOLLOWS:

1. This Bylaw including the attached Schedule "A" may be cited as the Town of Bonnyville Railway Area Structure Plan Bylaw.
2. The purpose of this Area Structure Plan (ASP) Bylaw is to refine or specify further the general policy direction and land use designations provided for these lands within the Town's Municipal Development Plan (MDP), to serve as the basis for land use classifications in the Town's Land Use Bylaw (LUB), and to establish a sound framework for future decisions on subsequent subdivision and development permit applications, and generally on the use of both the railroad right-of-way and the former station grounds, for the lands situated within the ASP boundary.

As attached, Figure 1 shows, the ASP area is basically a corridor - well-defined by the Town boundary on the east and west, 54 Avenue and Highway 28 on the south, and an industrial area to the north.

3. This Bylaw may be amended by Bylaw in accordance with the Municipal Government Act.
4. The Town of Bonnyville Railway Area Structure Plan Bylaw on the date of final reading of this Bylaw shall come into force and effect on that date.

INTRODUCED AND GIVEN FIRST READING this 12th day of June, A.D., 2001.

[Signature]
Mayor

[Signature]
Chief Administrative Officer

AS ADVERTISED on the 25th day of June, 2001 and the 2nd day of July, A.D., 2001 in the Bonnyville Nouvelle.

PUBLIC HEARING HELD AT THE TOWN HALL COUNCIL CHAMBERS on the 10th day of July, A.D., 2001.

[Signature]
Mayor

[Signature]
Chief Administrative Officer

GIVEN SECOND READING this 10th day of July, A.D., 2001.

[Signature]
Mayor

[Signature]
Chief Administrative Officer

GIVEN THIRD AND FINAL READING this 9th day of October, A.D., 2001.

[Signature]
Mayor

[Signature]
Chief Administrative Officer

Schedule "A"
to
Bylaw No. 1194-01

**RAILWAY LANDS
AREA STRUCTURE PLAN**

TOWN OF BONNYVILLE

2001

October 9, 2001

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RAILWAY LANDS AREA STRUCTURE PLAN

TOWN OF BONNYVILLE

2001

PART I - INTRODUCTION

1.0 Planning Background

The Town of Bonnyville is located in the Lakeland region of northern Alberta, 250 kilometers northeast of Edmonton. The economic base of Bonnyville has traditionally been agricultural service oriented, but most of the Town's more recent growth has been directly attributable to heavy oil exploration and development. Bonnyville's planning framework envisages the community moving from a resource-based economy to a sustained economy based on diversification. Currently, the Town is in the midst of an economic upturn due to oil exploration and development.

The Town has been planning actively for a long time. A General Municipal Plan was first adopted in 1979. New General Municipal Plans were prepared and adopted in 1986 and again in 1992. Specific land use controls were first implemented through a Land Use Bylaw, adopted in 1979. A new Land Use Bylaw was adopted in 1994.

A new Municipal Development Plan, prepared to satisfy the requirements of the Municipal Government Act, 1994, as amended, was prepared and approved in 1997. A new Land Use bylaw followed in 1998.

2.0 Need for an Area Structure Plan

The Town's 1997 Municipal Development Plan provided a great many good policies for land use and development in the Town.

However, since the approval of the Municipal Development Plan (MDP), Canadian National Railways (CNR) has announced that it will be removing the rail line from its right-of-way and disposing of its land holdings in the area of the rail line, including the right-of-way itself. Upon review by Town Council, it was determined that the policies of the MDP do not specifically deal with redevelopment of the railroad right-of-way. The MDP presumes the rail line would still operate, and the lands adjacent to the rail line would be used for various related business purposes. The MDP provides only general development policies for the rail line area.

As well, there is a move to have a public agency (a cooperative venture of several municipalities in the Bonnyville region) acquire the railroad right-of-way itself acquired for the purposes of developing both a "trail" and a utility corridor and/or reestablishing a railway line through the Town. This act would also have implications on redevelopment of these rail-related lands.

Therefore, it was decided that a more detailed land use and development plan, an Area Structure Plan (ASP), should be developed to provide more detailed land use and development policies and guidelines for the affected area.

This ASP, known as the Railway Lands Area Structure Plan, has been prepared pursuant to and complies with the relevant provisions of the Municipal Government Act, 1994, as amended.

The Railway Lands Area Structure Plan applies to an area of land situated within the Town of Bonnyville, as shown on Figure 1, the Location Map.

This is the former right-of-way and station grounds of the CNR line in Bonnyville, together with some adjacent lands.

3.0 Purpose of Area Structure Plan

The purpose of this ASP is to refine or specify further the general policy direction and land use designations provided for these lands within the Town's MDP, to serve as the basis for land use classifications in the Town's Land Use Bylaw (LUB), and to establish a sound framework for future decisions on subsequent subdivision and development permit applications, and generally on the use of both the railroad right-of-way and the former station grounds, for the lands situated within the ASP boundary.

As Figure 1 shows, the ASP area is basically a corridor - well defined by the Town boundary on the east and west, 54 Avenue and Highway 28 on the south, and an industrial area to the north.

4.0 Planning Framework

Section 633 of the Municipal Government Act, 1994, as amended, provides for the preparation of Area Structure Plans by a municipality. This Area Structure Plan has been prepared in conformity with that Act.

The Town of Bonnyville Municipal Development Plan designates almost all of the area affected by the Railway Lands Area Structure Plan as "Industrial/Commercial." Various industrial and commercial land use and development policies apply.

Both 54 Avenue and Highway 28, which border the ASP area, and both 46 Street and 55 Street, which traverse the ASP area, are designated "Existing Arterial" roadways in the MDP. Again, various transportation-oriented policies apply, some of which specifically relate to the matter of traffic flow and use of 54 Avenue as a "Truck Route," allowing large trucks a way around the main Downtown Commercial Core area of the Town.

And therein lies the crux. Before the ASP development exercise began, the Town indicated that it had no special concerns with the future use of the affected land being commercial or industrial in nature. The land is ideally situated for such uses, both in relation to other uses and in relation to transportation facilities.

However, because of the impact uses might have on both the Downtown Commercial Core and on the viability and usefulness of 54 Avenue as a truck route, and because, if Canadian National Railways disposes of its land holdings in the area, instead of having one landowner, but, potentially, several, Council decided that more detailed policies, outlining the matters which will be specially attributable to development in the Railway Lands area should be developed and applied to the Railway Lands.

5.0 Description of Property

The ASP area contains approximately 20 ha (45 ac.) of land.

The area is generally flat and very gently slopes to the north and east. The majority of the ASP area is either vacant, former station grounds, or industrial land. The Bonnyville and District Historical Society operates a museum on lands it owns.

The railroad right-of-way was established many years ago. In 1994, CNR subdivided out a 15-m (50 feet) wide strip of land on the north side of the railroad right-of-way (Plan 942-2711) and sold it privately. In 1995, the area south of the railroad right-of-way was subdivided into several individual lots (Plan 952-0604). In 1999, two further areas were subdivided from the old railroad plan (Plan 992-6365), including the "Y" north of the rail line and touching on 56 Avenue.

Drainage from the ASP area generally is from the south west to the north east; however, the railroad line itself acts to some extent as a barrier to drainage.

There are no unique or unusual features about the land that would require special planning consideration except for the unusual dimensions of most of the properties.

Plan 942-2711 defines a property that is almost 1.6 km (1 mile) long, but only 15 m (49.2 ft.) wide. This property is also physically fragmented into two parts, though it is held in one title.

The railway right-of-way (held in two Plans) is, for the most part, 30 m (98.4 ft.) wide, and almost 3.2 km (2 miles) long.

Most of the properties located south of the railroad right-of-way (Areas 1 to 7, Plan 952-0604 and Lot 8, Plan 992-6365) are only about 40 m (131 ft.) deep, and the "Y" (Lot 9, Plan 992-6365) presents its own problems in dimensions and shape.

This short depth is further exacerbated by the adjacency of 54 Avenue, which is labeled a "Truck Route." It is thus identified as a road along which the Town wishes and expects considerable heavy traffic to be directed. However, the road right-of-way is only 20.12 m (66 ft.) wide. There is not enough room in this right-of-way to provide the standard carriageway for the type and volume of traffic proposed. Widening 54 Avenue to the south is not possible due to the even shorter depth of most of the lots fronting on the south side of 54 Avenue. However, widening 54 Avenue to the north can only further reduce the depth of any properties on the north side of 54 Avenue.

These unusual and difficult shapes and dimensions pose special problems, severely limiting development.

The lands were used for several decades as rail lands and as a railroad station ground. Undoubtedly, there were some chemical spills on the land. As a result, environmental assessments are being done on these lands.

6.0 Land Ownership

The ASP area's approximate 15.5 ha (38 ac.) are held under 12 separate titles. Most of the land used to be part of the railway right-of-way and station grounds associated with the railway right-of-way in two Plans, Plan 1210EO and Plan 3592EO.

The lands are currently held as follows:

<u>Plan</u>	<u>Block Lot</u>	<u>Area</u>	<u>Owner</u>
Pt. 1210EO	Railway lands	not known	Canadian National Railway
Pt. 3592EO	Railway lands	not known	Canadian National Railway
942-2711		2.0555 ha (5.078 ac.)	Helen Vachon
952-0604	Area 1	0.737 ha (1.82 ac.)	Petro-Canada
952-0604	Area 2	1.07 ha (2.64 ac.)	Canadian National Railway
952-0604	Area 3	0.175 ha (0.43 ac.)	Imperial Oil Limited
952-0604	Area 4	1.54 ha (3.81 ac.)	Canadian National Railway
952-0604	Area 5	1.02 ha (2.52 ac.)	Canadian National Railway
952-0604	Area 6	0.943 ha (2.32 ac.)	Canadian National Railway
Pt. 952-0604	Area 7		Canadian National Railway
992-6365	8	0.54 ha (1.33 ac.)	Canadian National Railway
992-6365	9	2.03 ha (5.02 ac.)	Canadian National Railway

The ownership pattern is shown on Figure 2.

The railroad right-of-way, the "Y", and most of the properties south of the railroad right-of-way are owned by CNR. The 15-m (50 feet) wide strip of land on the north side of the railroad right-of-way is privately owned. Two of the lots south of the railroad right-of-way are owned by Imperial Oil and to Petro-Canada. Both Petro-Canada and Imperial Oil own lands which have historically been used by them (or by predecessor companies) as oil tank and oil storage sites, and they intend to continue to own these sites, thus limiting subsequent users and the environmental impact on any subsequent users.

7.0 Adjacent Development

This ASP falls within a well-planned community. As such, development has already been planned and appropriate land use and development policies are already in place for lands surrounding the ASP area.

Almost all of the lands both north and south of the ASP area are designated "Industrial/Commercial" in the Town's MDP. Three areas - north of the ASP area to the west of 55 Street, south of 54 Avenue to the east of 55 Street, and south of 54 Avenue at the Duclos School - are designated "Residential."

Almost all of the lands designated "Industrial/Commercial" are either developed in business uses or are vacant. There are three small, older houses on the south side of 54 Avenue in the 50/51 Street area. The areas designated "Residential" are occupied by dwellings or by the Duclos School playground.

A key significance to the development of the ASP area lands is that almost all of them lie immediately adjacent to the Town's Truck Route. As such, they are highly visible to vehicular traffic and to tourist traffic.

PART II - DEVELOPMENT CONSTRAINTS

1.0 Positive Attributes of Lands in the Plan Area

There are four basic positive attributes to lands in the ASP area which will encourage quality development in the ASP area.

The first is the proximity, and thus the visibility and apparent accessibility of the lands to 54 Avenue, the Town's Truck Route.

Another basic positive attribute is that most of the lands are under one ownership (though already subdivided into many parcels of land).

A third basic positive attribute is that most of the lands are vacant. There is very little capital inertia inhibiting development or redevelopment of most of the lands.

The final basic positive attribute is that the lands provide a 3.2 km (2 mile) long corridor across the Town - a corridor which may be able to be used for any number of uses that require or benefit from a corridor's shape.

2.0 Negative Attributes of Lands in the Plan Area

There are two basic negative attributes to lands in the Plan area which would discourage quality development in the Plan area.

The first basic negative attribute is the narrowness of the Plan area itself, and of many of the properties within the Plan area. Though 1.6 to 3.2 km long, the properties are only 15 m, 30 m or 40 m wide (or deep, depending on how one looks at it). Such dimensions substantially reduce development potential.

The second basic negative attribute is, paradoxically, the same as one of the positive attributes - proximity to 54 Avenue (the Truck Route). Since 54 Avenue is a Truck Route, the Town wants, quite rightly, to control access onto the road so as to maintain or enhance its use as a high volume, large vehicle roadway. A large number of accesses onto 54 Avenue will significantly impact its ability to fulfill the mandate that Town Council has determined it should have.

3.0 External Influences and Overriding Considerations

In considering development of the ASP area, there are a number of external influences and overriding considerations affecting this area.

First, the corridor presents a unique opportunity as a "trail", a pedestrian and small vehicle corridor through the northern part of the Town. The Bonnyville and District Historical Society already operates a museum and interpretive center adjacent to the rail line and within the ASP area.

Second, the corridor also presents a unique opportunity as a utility corridor through the northern part of the Town. The Town will, in the future, need to extend service pipelines from the east to west part of the Town. The corridor can be used in that respect.

Third, the Town's policy, as articulated in the MDP, is to enhance the Downtown Commercial Core and to do nothing to reduce the viability of the Core. Commercial development adjacent to

54 Avenue in the ASP area, if not carefully controlled, has the potential of seriously negatively impacting the viability of the commercial enterprises in the Downtown Core. Any business which takes away from business in the Core would be a detriment. Yet, almost any business will take away business from the Core. The key will be to attract to this corridor businesses that would, when all factors are taken into account, either add to or not take away anything from the Core's viability.

Fourth, the corridor currently acts as a barrier to access to and from the lands to the north of the railroad right-of-way from the core part of the Town. Currently, only 46 Street and 55 Street cross the railroad right-of-way. With redevelopment of the ASP area, there may be an opportunity to have another street, perhaps 50 or 51 Street, cross the lands to provide more direct access between the Downtown Core and the industrial lands north of the ASP area.

Fifth, in a consortium with other municipalities, the Town is working toward having a publicly-owned agency acquire the railroad right-of-way and the station grounds from CNR. Alternatively, the Town may acquire the right-of-way and station grounds. If the lands are acquired in this manner, development of most of the lands in the ASP area can be more completely controlled by the Town directly or through the consortium. However, even if the lands are not acquired by the Town or by the consortium, the Town can and will apply appropriate development policies, regulations and controls on development and redevelopment of the ASP area's lands.

4.0 Narrowness of the Corridor and the Need to Widen 54 Avenue

Finally, the narrowness (or shallowness, depending on one's point of view) of the properties and the need to widen 54 Avenue so as to be able to have it function as a Truck Route rides over any land use decisions and severely impacts the ASP area's lands.

The right-of-way width of 54 Avenue is only 20.12 m (66 ft.). It is currently constructed as a wide, 2-lane roadway.

In order to properly operate as a Truck Route, 54 Avenue should have four lanes, with right and left turn lanes at appropriate locations. Current plans call for traffic signals to be installed on 54 Avenue at 55 and 46 Streets, as well as the current location where 54 Avenue meets Highway 28 in the east part of Town. If either 50 or 51 Street is extended through the ASP area from 54 Avenue to the north, traffic signals may be advisable at that location as well. Right and left turn lanes should be provided at all traffic signals to enhance traffic movements.

The normal standard for development of a 4-lane roadway, providing allowances for left and right turn lanes, is between 36 m (118 ft.) and 45 m (147.6 ft.). Those widths are not feasible here. Requiring them would reduce the depth of the properties on the north side of 54 Avenue to a completely untenable amount.

In fact, however, a full, 4-lane roadway, with left and right turn lanes, can physically be constructed within a width of 22 m (72.2 ft.). That width, of course, does not meet normal standards. It would provide no room for any utilities, for any sidewalks, or for any other street facilities. However, 54 Avenue is a problem, and innovative solutions must be sought and determined.

A further consideration which affects 54 Avenue concerns the current use of the 54 Avenue right-of-way. A considerable amount of land is located between the south side of the right-of-way boundary and the actual 54 Avenue roadway. Several landowners and businesses on the south side of 54 Avenue have come to use that part of the right-of-way for the storage of goods

and materials, and for parking and loading areas. When a new 54 Avenue roadway is constructed, if a minimal amount of land is to be taken off the lands on the north side of 54 Avenue, the maximum amount of land on the south side of the right-of-way of 54 Avenue will have to be developed for roadway, and the use by the adjacent landowners and businesses will have to stop. This will cause disruption to some landowners and businesses; however, it should be noted that they are currently using the public road right-of-way for their own private purposes.

5.0 Plan 942-2711

Plan 942-2711 presents its own special problems and special opportunities.

This area was carved out of the station grounds by CNR in 1994 and sold to a private party. It is one lot (and, therefore, one title) 15 m wide and almost 1.6 km long. The lot is fragmented into two separate areas - one fronting on 55 Street and the other fronting on 46 Street. It is almost entirely undevelopable in its present form.

At the same time, the area south of the railroad right-of-way is so narrow (or shallow) that these lots are also very difficult to develop. This difficulty will be exacerbated by any road right-of-way widening acquired to provide for a widened 54 Avenue (Truck Route).

A potential solution to this mutual problem is for the owner of the railroad right-of-way and most of the lands south of the right-of-way, whether it is CNR, the Town, or the consortium referred to elsewhere in this ASP, to encourage the owner of Plan 942-2711 to enter into a resubdivision of the area. In the old, pre-1995 Planning Act, it would have been called a "replotting scheme." Plan 942-2711 would be absorbed into the area, the trail and utility corridor would be moved to the north, 54 Avenue would be widened, the lots south of the corridor would be made deeper, and the owner of Plan 942-2711 would be given compensation, either in the form of money or consideration towards a lot on the south side of the corridor.

6.0 Principles of the Development Concept

Development of the land in the ASP area will take the form indicated in Figure 3.

First and foremost, a trail and utility corridor will be provided through the ASP area, running from the eastern to the western extremity of the ASP area. This corridor will be used for a pedestrian and small scale vehicle (such as bicycles and snowmobiles) trail, for uses accessory to the use of the trail, for rest areas, for interpretive centers, and for utilities which have to cross the Town.

Second, for the most part, the predominant use of the rest of the land in the ASP area will be commercial, though industrial uses will be allowed. The commercial uses allowed will be those which serve the trail traveling public or the trucker, but not the common car-traveling public or the Town or region at large. The commercial uses approved will specifically be those which will complement rather than negatively impact uses in the Town's Downtown Commercial Core. As well, the commercial developments will be those which make good use of the narrow sites available.

Industrial uses will be limited to those which do not negatively impact the quality of the environment in any way.

Outdoor storage will be strictly prohibited in the ASP area.

Third, the roadway pattern.

54 Avenue will be minimally widened, then rebuilt to provide for four lanes of traffic, with right and left turn lanes at specific locations to ease the potential for traffic congestion. This will reduce the need to widen 54 Avenue except minimally. The use of the 54 Avenue right-of-way by landowners and businesses on the south side of 54 Avenue will be curtailed.

Accesses from 54 Avenue to the parcels of land on the north side of 54 Avenue will be limited to locations opposite the Streets which approach 54 Avenue from the south.

Fourth, a resubdivision will be attempted.

A resubdivision of the lands north of 54 Avenue between 55 and 46 Streets will be encouraged to consolidate Plan 942-2711 into the development area, to align parcel boundaries on the north side of 54 Avenue with the Streets opposite, to move the trail and utility corridor to the north, to deepen the parcels of land on the north side of 54 Avenue, and to provide a widening of 54 Avenue.

PART III - DEVELOPMENT CONCEPT DETAILS

1.0 Land Uses

Development of the land in the Plan Area will take the form indicated in Figure 3.

A trail and utility corridor will be provided through the Plan area, running from the eastern extremity of the Plan area to the western extremity. This trail and utility corridor will vary in width, but will be predominantly 30 m wide. This will provide enough room for parallel pedestrian and small scale vehicle (such as bicycles and snowmobiles) trails, for small rest areas, and for small interpretive centers.

Utilities and their facilities, such as major trunk pipelines, will be placed underground within the trail area.

For the most part, the predominant use of the rest of the land will be commercial, though some industrial uses will be allowed. The commercial uses allowed will be those which serve the trail user or the trucker, but neither the general car traveler or the community or region at large. The commercial uses approved will specifically be those which will complement and not directly compete with uses in the Town's Downtown Core. As well, the commercial developments will be those which make good use of the narrow sites available.

Trail uses, including uses which are accessory to the trail use or serve the trail-using public, will also be allowed - with the limitations to commercial uses as indicated above.

Industrial uses will be limited to those which do not negatively impact the quality of the environment in any way. For instance, manufacturing will be limited to small scale operations which are conducted entirely indoors and which have no external emissions of any kind.

Outdoor storage, even if it is part of a manufacturing, a warehousing, or even a retail operation, will be strictly prohibited in the Plan area as there is no effective way to screen such storage in the close confines provided by the narrow Plan area.

2.0 Land Use Bylaw

In order to implement these policies, the existing Direct Control District in the Town's Land Use Bylaw currently applied to these lands will be retained for the lands on the north side of 54 Avenue, the trail/utility corridor, and Plan 942-2711 in the Plan area. This District will allow Council to carefully control land use and development so that it conforms with the intent of this Plan. The key to approving a development within the Plan area will be a determination of whether the proposed development and use, taking into account its size, location, type, and services, will complement rather than compete with the Downtown Commercial Core and will directly serve the truck and trail traffic on 54 Avenue and on the trail system. Developments that would compete with the Downtown Commercial Core will not be approved.

As well, implementation of the Direct Control District will allow for adequate setbacks for developments, especially yards to provide for the eventual widening of 54 Avenue and to provide for a good separation of the developments from the trail corridor. Since the 54 Avenue roadway is to be constructed within a narrow right-of-way, there must be adequate setback for developments from the roadway. This will limit the developability of the lands adjacent to the north side of 54 Avenue.

The Direct Control District's implementation will also provide for careful consideration of the aesthetics of the development form, including the massing of walls, colour, visibility, perception, and the like, so as to minimize the impact of developments on the view of the Town from both the trail corridor and from 54 Avenue. Signage will likewise be carefully controlled for the same reasons.

The "Y" will be placed in the General Industrial (M-1) District in the Land Use Bylaw. Though access to the "Y" is limited, it does have access and can be developed for appropriate industrial uses.

3.0 Roadways

The right-of-way of 54 Avenue will be widened to 27.0 m (88.6 ft.), and 54 Avenue will be rebuilt to provide for four lanes of traffic, with right and left turn lanes only in specific locations necessary to ease the potential for traffic congestion. Almost the whole of the right-of-way of 54 Avenue will be used for this roadway. Utility services will either be moved underground, or provided in the lane south of the lots fronting on 54 Avenue (or on 53 Avenue where lots "double front"). When 54 Avenue is rebuilt, the current use of the 54 Avenue right-of-way by landowners and businesses on the south side of 54 Avenue will be strictly curtailed.

Construction of these roadways will be budgeted within the Town's 5-year capital plan.

4.0 Accesses

Accesses from 54 Avenue to the parcels of land on the north side of 54 Avenue will be limited to locations opposite the Streets which approach 54 Avenue from the south. In other words, accesses to the north side of 54 Avenue will be at the same location as the Streets on the south side of 54 Avenue.

The lands between 54 Avenue and the railroad right-of-way are already subdivided into individual titles, and, in three instances, are actually owned by parties other than CNR or the Historical Society. Nevertheless, the access policy will be followed as much as possible.

When development is proposed for a parcel of land on the north side of 54 Avenue, the only accesses onto 54 Avenue will be located opposite an existing Street. As well, as a condition of the development of the parcel of land, access easements in favour of adjoining parcels of land, which may have more limited or more difficult accesses, will be required by the Town. These access easements will be mutual, and will lessen the dependency for multiple direct access onto 54 Avenue.

5.0 Resubdivision

Strictly speaking, after making a careful assessment of the lotting situation on the north side of 54 Avenue, no alteration to parcel boundaries is necessary. Access to each of the existing parcels, except for the first and third parcels east of 55 Street, can be provided opposite existing Streets. However, the landowners will be encouraged, wherever possible, to adjust the boundaries of the parcels of land on the north side of 54 Avenue so that they line up with the streets on the south side of 54 Avenue.

As well, while boundary adjustments are done to resolve access problems on the north side of 54 Avenue, the majority landowner, whether CNR, the Town, or the consortium, will be encouraged to rectify the depth concerns of the parcels adjacent to 54 Avenue. Specifically, CNR exacerbated the problem of the depth of the lots in 1994 when the 15-m wide and 1.6 km long strip of land north of the railroad right-of-way was subdivided off, without Subdivision Authority or Town approval, and sold to a private party. CNR or the consortium will be encouraged to approach the owner of the 15-m wide parcel (Plan 942-2711), acquire it, and consolidate it with all the lands it can in the ASP area between 55 and 46 Streets. The resulting lands will be about 85 m (278.9 ft.) wide from 54 Avenue to the Public Utility Lot on the north side of the ASP area and on the south side of the industrial subdivision on the north side of the ASP area. A resubdivision of the lands can then be effected, to move the trail and utility corridor to the north so that it occupies the northerly 30 m of the combined lands. After a small widening of 54 Avenue (7.0 m – 22.6 ft.), the lot depth will be 48.0 m (157.5 ft.), still narrow, but much better than would exist without the consolidation.

The owner of Plan 942-2711 will be advised that this will be an opportunity to realize a good return from his investment. The parcel of land is only 15 m (49.2 ft.) wide. It cannot be further subdivided except into two shorter, but still narrow, parcels, since it has no access to any road other than 46 Street at its east end and 55 Street at its west end. However, even if it were subdivided, the only development that could occur on it would be substantially restricted by the width of the parcel.

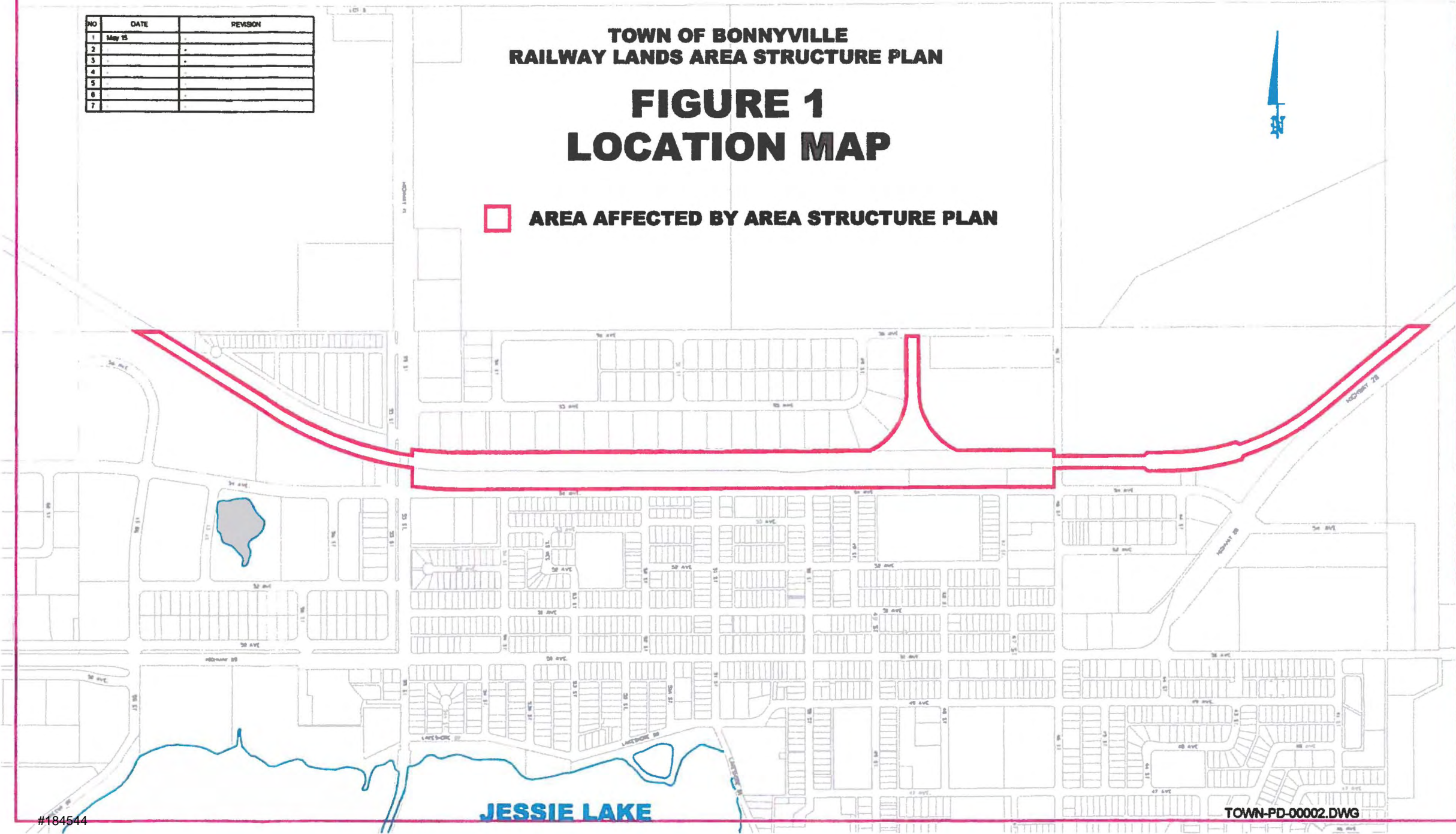
Realignment of the boundaries of the "Y", especially the eastern boundary, with adjacent properties will be encouraged so as to make the property more regular in shape, should the land owners wish.

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**TOWN OF BONNYVILLE
RAILWAY LANDS AREA STRUCTURE PLAN**

**FIGURE 1
LOCATION MAP**

 **AREA AFFECTED BY AREA STRUCTURE PLAN**

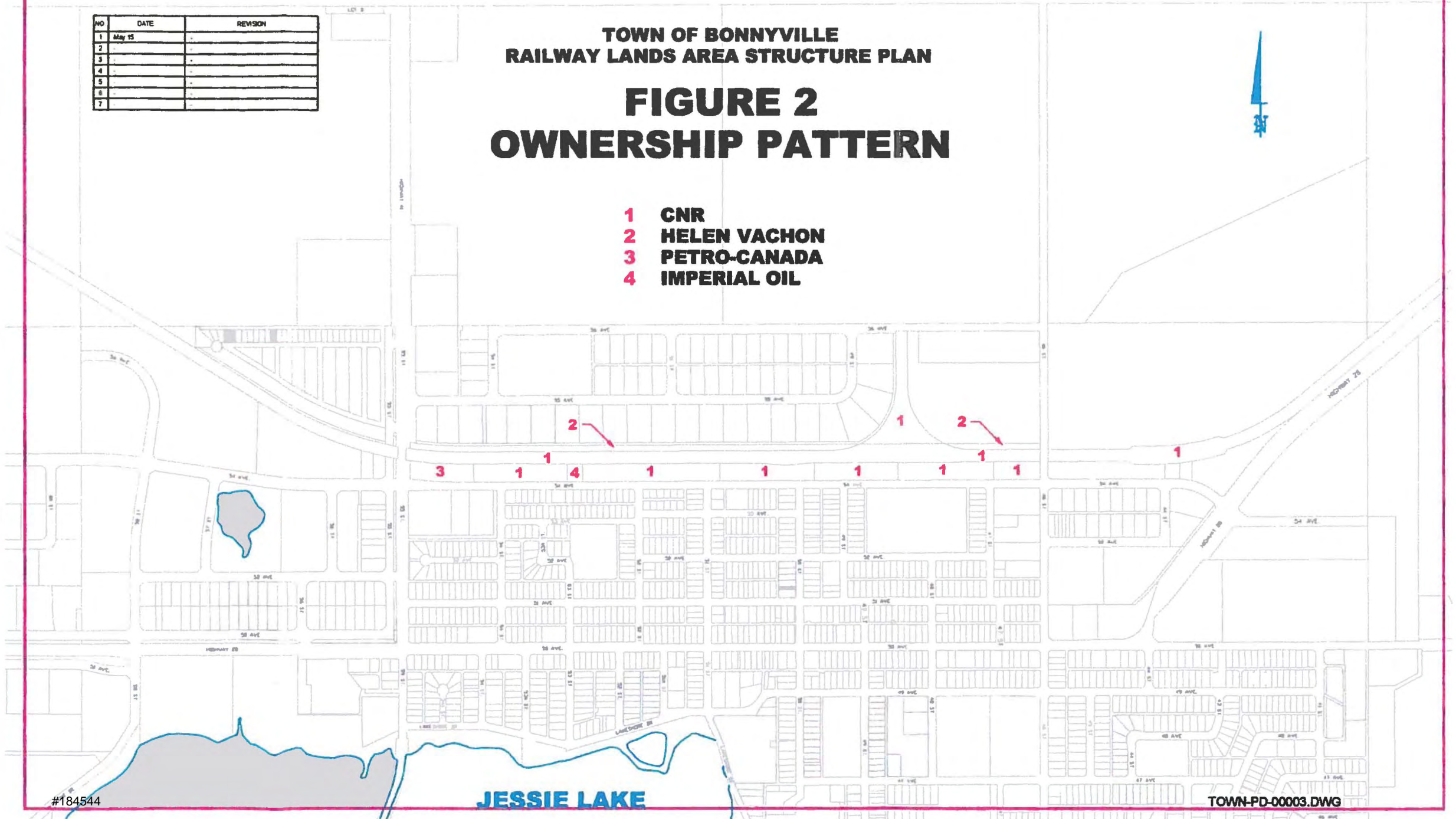


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**TOWN OF BONNYVILLE
RAILWAY LANDS AREA STRUCTURE PLAN**

**FIGURE 2
OWNERSHIP PATTERN**

- 1 CNR**
- 2 HELEN VACHON**
- 3 PETRO-CANADA**
- 4 IMPERIAL OIL**



NO	DATE	REVISION
1	May 15	
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**TOWN OF BONNYVILLE
RAILWAY LANDS AREA STRUCTURE PLAN**

**FIGURE 3
DEVELOPMENT CONCEPT**

- **TRAIL & UTILITY CORRIDOR**
- ↑ **ACCESSES TO 54 AVENUE**
- Y **THE "Y"**

