

5.2.2 Commercial Policies

The MDP also identifies part of the plan area as Commercial East and outlines relevant policies. Among the requirements are that new development for this area must be evaluated according to its impact on adjacent land uses and its influence on the vitality of existing uses such as: the downtown commercial area; complementing the Centennial Centre; its relationship to the proposed residential development to the south and east; and the redevelopment of the Muni Cor lands. Generally, the area's aesthetic considerations should be given a high priority given its prominent location at Bonnyville's eastern entrance.

The MDP policy context allows the opportunity to develop commercial lands that contribute to the Town's stated vision. Development should occur in a manner that provides an appropriate transition from commercial to planned residential uses while seamlessly integrating access to the local population. As such, site design should be done in such a manner as to serve local residents and businesses while attracting other regional residents and businesses to the area. Local residents may be encouraged to walk, bike or drive shorter distances to acquire neighbourhood commercial services. Commercial sites may in effect offer spaces for gathering to community residents and visitors to the plan area.

Objectives:

- To provide a range commercial uses oriented to the needs of local and regional residents and visitors;
- To ensure high quality building design, detailing, signage and landscaping for all commercial development that reinforce and promote architectural aesthetics to facilitate the development of the plan's commercial area as a regional attraction, while accommodating access for local residential communities. The developer reserves the right to articulate further site specific design guidelines through a restrictive covenant on the affected land parcel;
- Facilitate the development of pedestrian links between commercial and other land uses where appropriate; and
- Place entry signs at specific entry points to the commercial area(s) to create a sense of identity, particularly on the northeast corner entering into town.

Mixed Use Residential/Commercial

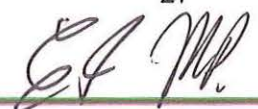
Part of the plan area has been identified for mixed residential/commercial uses allowing the potential for residential and commercial uses to co-exist within one site. The areas are generally located along the boundary of existing development; and provides a land use transition necessary by offering ground level commercial with residential uses above. An appropriate designation for the area could be CRX-Commercial Transition District or may require the design of new direct control district depending on market conditions.

Commercial Design Guidelines

- Entryway design elements and variations should give orientation and aesthetically pleasing character to buildings;
- All facades of a commercial building that are visible from residential properties and/or public streets shall ensure a pleasing scale and feature characteristics similar to the front of adjacent façades;
- Buildings should be provided with a distinct "base" through the use of materials, texture or massing to create landmark structures with distinctive roof forms, decorative structures, and unique geometry;
- Buildings should provide generous amounts of windows, skylights, or similar natural-light producing products to create ground floors with a "transparent" quality;
- Buildings should provide protection from adverse weather conditions and utilize overhangs, marquees, and awnings at entrances and loading zones;
- Landscape and buffering should contribute to visual quality and continuity within and between developments, provide screening and mitigation of potential conflicts between activity areas and site elements, enhance outdoor spaces, reduce erosion and stormwater runoff and mitigate air pollution;
- Off-street parking for commercial buildings should be designed to minimize visual impact;
- Site access should be encouraged using collector roads; and
- Parking lots should be configured and designed to reduce the overall mass of paved surfaces. No more than 50% of an off-street parking lot for the entire property should be located between the front façade of the principal building(s) and the adjacent public street.

5.2.3 Institutional

The plan area retains one site for institutional uses to provide for the existing place of worship. Such sites can provide valuable cultural, social and/or health services to residents of the local community and surrounding areas. By locating such amenities within close proximity to residential neighbourhoods they can offer opportunities to maximize the delivery of specialized services. In this instance, the current institutional uses are considered complimentary for neighbouring residential and commercial development.



Objectives:

- To permit institutional land uses in designated portions of the plan area to be further expanded and intensified;
- The Town may require institutional uses that generate high volumes of vehicle traffic to provide additional access through a collector road to reduce traffic concerns within the plan area. The Development Authority may require the preparation of a Traffic Impact Assessment (TIA) as part of future development permit applications;
- To reduce the overall mass and visual impact of paved surfaces, parking lots should be configured and designed to no more than 50% of an off-street parking lot for the entire property should be located between the front façade of the principal building(s) and the adjacent public street;
- Landscaping and buffering should contribute to the visual quality and continuity within and between developments, provide screening and mitigation of potential conflicts between activity areas and impacts on adjacent properties. Particular regard for: site elements, enhancing outdoor spaces, reduce erosion and stormwater runoff and mitigation of air pollution; and
- Site design should ensure that orientation and placement of buildings alleviates potential privacy and noise concerns and is sufficient for the provision of off-street parking.

5.2.4 Public Open Space and Environmental Reserve

Bonnyville's commitment to a high quality of life and sense of place are created, in part, by the town's natural setting, parks, and trails. The plan area contains the Centennial Centre lands which are the Town's only large nonschool park space and have recently undergone extensive redevelopment. It is the Town's desire to create a fully linked open space system that includes trails, public parks, natural open spaces, and school sites.⁴

The plan area includes 83.52 hectares of developable land of which up to 10 percent may be collected through Municipal Reserve (MR) dedication by the Town of Bonnyville as permitted by the Municipal Government Act. The Town may use land obtained through MR dedication for the purpose of establishing public parks, recreation areas, trails and land use buffers, or provide the land to the local school authority for the purposes of creating a school site.

Ten percent of the developable area is 8.35 hectares. Based on information obtained from land titles 1.13 hectares of land has been deferred through caveat from developed parcels within the plan area. Therefore, a total of 9.48 hectares of land is required for MR within the plan area. At this time 8.66 hectares is proposed for MR dedication, while the remaining 0.82 hectares will be further defined through the submission of outline plans. Neighbourhoods outlined for single

⁴ Town of Bonnyville, Municipal Development Plan, pg. 32.
SE Design and Consulting Inc.



family and multifamily uses may be required to provide a "tot lot" amenity as described in the MDP. Alternatively, cash in lieu of MR may be provided at the time of subdivision.

See *Table 3 – MR Dedication Summary*, detailed Municipal Reserve area calculations are available in Appendix VII.

Development of the RMH area will be required to provide 5.0% of amenity park space within the neighbourhood and will be conceptualized through an outline plan to be provided at the time of



Illustration 1 - Concept of the East Gate linear walkway proposed adjacent to the highway commercial uses and residential areas depicting the use of a meandering pathway flanked by undulating berm and tree planting.

development approval. Areas used for public park spaces are located and designed to create an interconnected open space system of parks, trails, and natural features. The intent is to link residential areas to existing or planned park, community facilities and other recreation areas, both within the proposed development and surrounding lands.

The planned community of East Gate will include the development of a neighbourhood park/school site as described in appendix 4 of the MDP. The plan provides 3.55 hectares of usable space for neighbourhood park or school purposes which is slightly below the minimum of 4.0 hectares recommended in the MDP; however, it is assumed that any additional outdoor amenity space could be augmented by the adjacent Centennial Centre lands. The site will

provide ample opportunity for the Town and school authorities to jointly consider suitable options for the site and negotiate joint-use agreements. The development could target elementary or high school age students; tots under supervision of adults; and residents of the neighbourhood. Informal activity areas that could be included are: walking and jogging trails; play structure activity; and space for youth gathering.⁵ An additional 1.5 hectares of park space is located adjacent to the Environmental Reserve designate along Barreyre Lake to provide visibility, access and a pedestrian linkage to this prominent natural feature. The site provides an alternate park space with some distance from the Centennial Centre lands and has clear visibility from its flanking street as illustrated in *Figure 8 - Development Concept*. Additionally, the ASP provides an alternate scheme for municipal and school reserve locations through *Figure 8a-Development Concept* depending on the emerging needs of the Town and School Authorities. Both schemes provide an integrated hierarchy of active and passive parks and recreation facilities to meet the diverse needs of Bonnyville's population. Based on the proposed population density and the unique location of the East Gate ASP area, residents will be adequately served by existing and proposed parks, open spaces and recreation facilities. The parks and open space linkage system is conceptually illustrated in *Figure 9 - Transportation Network*.

Moreover, the plan proposes the design and development of a boulevard on the north side of 50th Avenue to create a unique community feature that contributes to improved visibility and enhances 50th Avenue as a major arterial roadway located on the east end of town.

The boulevard adds to a sense of place for local residents and emphasizes visually the importance of the Centennial Centre for the town. Refer to *Figure 8 - Development Concept* for an illustration of the boulevard location and Illustration II for a conceptual drawing of the amenity. All landscaping shall be developed to the satisfaction of the Development Authority. Pedestrian linkages and trails are further elaborated in section 5.3.2.2 below.

Objectives:

- To expand the parks and open space system to link into new residential, commercial, and industrial areas from existing residential neighbourhoods;
- Plan and develop a variety of open space opportunities ranging from formal parks, open spaces, trails and natural areas adjacent to Barreyre Lake; and
- Apply relevant C.E.P.T.D. principles for park layout, design and landscaping.

⁵ Town of Bonnyville, Municipal Development Plan, Appendix IV

*A detailed version of this table is provided in Appendix VII.

** The plan proposes to use closure of road right of ways for portions of Range Road 240 along the east boundary for public Park and Open Space uses.

Table 3 MR Dedication Summary*

MR DEDICATION SUMMARY	
	Area (ha)
ASP development area	103.02
Net development area	92.44
Existing Park and Open Space	10.19
Calculated developable land	83.52
MR required for developable land	8.35
MR owing under Deferred Reserve Caveats	1.13
Total MR required for developable land	9.48
MR to be provided at the time of outline plan approval (or cash in lieu of MR at time of subdivision)	0.82
MR provided for development area	8.66
Additional Public land dedicated for Linear Open Space and Park Uses**	1.63
Total Park and Open Space within ASP area	21.30

Environmental Reserve

A total of 0.39 hectares of land has been identified for dedication as Environmental Reserve (ER) along the shoreline of Barreyre Lake.

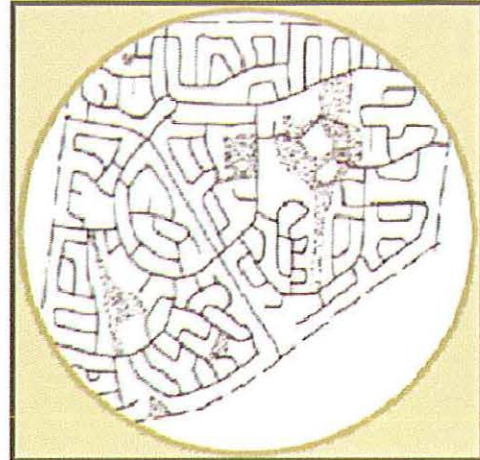
5.2.5 Storm Water Management

The overall scale of the development will require the design and construction of two storm water management facilities on site. Storm pond A will be located in the northern boundary of the area adjacent to the Highway 28; and pond B near 50th Avenue and the east side of the proposed road connecting to Beau Vista (south of the area outlined for RMH uses) within a low laying area. The developer shall be responsible for meeting any requirements of AENV, and/or other governmental agencies, and will have any changes reflected in the plan prior to subdivision. Sites identified for storm water managements facilities shall be identified as Public Utility Lots (PUL) at the time of subdivision. Further elaboration about storm water management is explained in section 5.3.1 Storm Drainage. The locations for storm water management facilities are illustrated in *Figure 10 - Storm Water Collection System*.

5.3 Infrastructure and Services

5.3.1 Circulation and Access

The plan area defines the location of arterial and collector roadways while local roadways will be designed through the submission of outlines plans for specific phases of the development. Local roads should be characterized by a curvilinear pattern combined with cul de sacs to create a more interesting streetscape. This coupled with a variety of lots sizes will craft a distinct neighbourhood which discourages through traffic in residential areas and directs major traffic onto arterial roads. The road design ensures adequate sight lines for both vehicular and pedestrian traffic. In addition, existing pedestrian circulation routes such as sidewalks and pathways will link into new land uses. The plan addresses predominantly two modes of transportation commonly used in Bonnyville: vehicular and pedestrian, which is explained as follows. Refer to *Figure 9 - Transportation Network* for an illustration of access points for each area.



5.3.1.1 Vehicular Transportation

The plan area is bound to the northwest by Highway 28, to the east by Range Road 240/34th Street, and by 50th Avenue/Highway 659 to the south. The proposed transportation network ensures that vehicular traffic is safely and efficiently moved within and among the various land uses, while discouraging shortcutting and high speed travel through local neighbourhoods. The plan includes submission of a Traffic Impact Assessment (TIA) carried out by Scheffer Andrew Planners and Consultants to determine the adequacy of the future transportation system and accommodate additional traffic generated by the proposed development. Four key intersections affecting the plan area were assessed for trip generation: Hwy 28 and 54 Avenue; Hwy 28 and East Gate Boulevard (34th Street); East Gate Boulevard (34th Street) and 50 Avenue; and an unnamed collector road (connecting south to Beau Vista) at 50th Avenue. The existing intersection of Highway 28 and 34th Street was not assessed at this time. Although, the plan proposes to close the intersection at this time, the location and alignment of this intersection should be reviewed at part of the future development of the NW 16-61-5-4.

Keys elements of the vehicular road layout are:

1. **Arterial Roads:** Designed to carry large volumes of traffic between areas in urban centres. Such roads often discourage residential entrances directly onto the road, carry traffic between neighbourhoods, and have major intersections with collector streets. Commercial areas are located along arterial roads. The MDP identifies the roads that define the plan boundary as either existing or proposed arterial roadways, with portions of Highway 28 designated for a Truck and Dangerous Goods route. The plan proposes realignment of Range Road 240/34th Street, tentatively identified in the plan as East Gate Boulevard, from its current location, which is further elaborated below. The intersection at Hwy 28 shall be retained, while a portion of Range Road 240 will be closed for use as a parkway. Please refer to point 3 in the next section for further clarification.
2. **Collector Roads:** Serve to provide quick access to the arterials from all local roads. New commercial developments, dwelling units and larger multifamily residential properties will be permitted to front onto collector roads. Three collector roads are identified for the plan area, refer to *Figure 9 - Transportation Network*.
3. **Local Roads:** While the overall arterial street pattern is based on a modified grid, the local streets should follow a curvilinear design. The general concept should include a curved street pattern that is characteristic in the adjacent neighbourhoods and often includes a large number of cul de sacs. The intent of the design is to channel traffic away from local streets to more busy and wider arterial roads, keeping through traffic out of local neighbourhoods to improve safety. Where appropriate, pedestrian linkages should be encouraged at the end of cul de sacs roads designs through linear parkways.
4. **Lanes:** The plan area will not permit single family residential subdivisions with lanes; however, individual property owners will have the option to include a front attached garage or not.
5. **Design Standards:** All road and sidewalk construction shall be according to Municipal Engineering Standards and detailed designs shall be submitted at time of development approval.



Road Layout

The East Gate ASP proposes to make the following modifications to the transportation system as defined in previous statutory plans and municipal transportation plans in order to enhance and make more effective the vehicular circulation system:

1. The proposed plan intends to align all roads with adjacent approved ASPs. Where necessary, appropriate connection points will be determined prior to subdivision with Alberta Transportation's approval.
2. A reconfiguration of the north south Range Road 240/34th Street segment from the 50th Avenue/Highway 659 intersection at the southeast corner of the East Gate ASP boundary to the Highway 28 intersection, tentatively named East Gate Boulevard. In order to improve the all-directional access at both the aforementioned intersections, the plan realigns the road on a northwest/southeast angle through the plan area, see *Figure 9 - Transportation Network* for an illustration of the described change. All road design changes shall be subject to approvals by Alberta Transportation and will be built according to the Municipal Servicing Plan and Engineering Design Standards. Although the new arterial road is labelled as East Gate Boulevard, all naming shall occur during the subdivision process, and shall be subject to the approval by the Town at such time.
3. The realignment of Range Road 240 will require portions of the road right-of-way to be closed along the plan's east boundary. The intersection of Highway 28 and Range Road 240 will be closed upon completion of appropriate roadway access points. Future intersection requirements at Highway 28 may be reviewed at the time of development within the NW 16-61-5-4, as illustrated in *Figure 8 – Development Concept*. The intersection lands will be integrated into the proposed road pattern under the new plan. The size of the retained road portion and the intersection shall be clarified prior to any approvals for road closure. The closed section of road shall be used as a linear parkway and act as a green buffer. The parkway should include a pathway to provide additional linkages to various community amenities and provide separation among existing and proposed land uses.
4. Internal road way pattern which consists of local roads will be detailed through outline plans and at the time of subdivision.



5. Among the key findings of the TIA is the need for revisions to traffic speeds, additional traffic signals and turn lanes at key intersections as development progresses. Refer to Appendix IV for the full traffic assessment, findings and recommendations. All road designs are subject to approval by Alberta Transportation at which time changes to Hwy 28 speed limits shall be addressed. The TIA forms part of the future approval requirements of the ASP. *Figure 9 - Transportation Network* illustrates and describes any key changes to the road system.
6. Based on the described phasing plan Range Road 240 will continue to function as a roadway until such time it is determined for closure. This road will continue to provide appropriate access to areas to the east of the plan area as development progresses. The road should not be closed until appropriate connections or turnarounds are constructed on all local and collector roads going east.

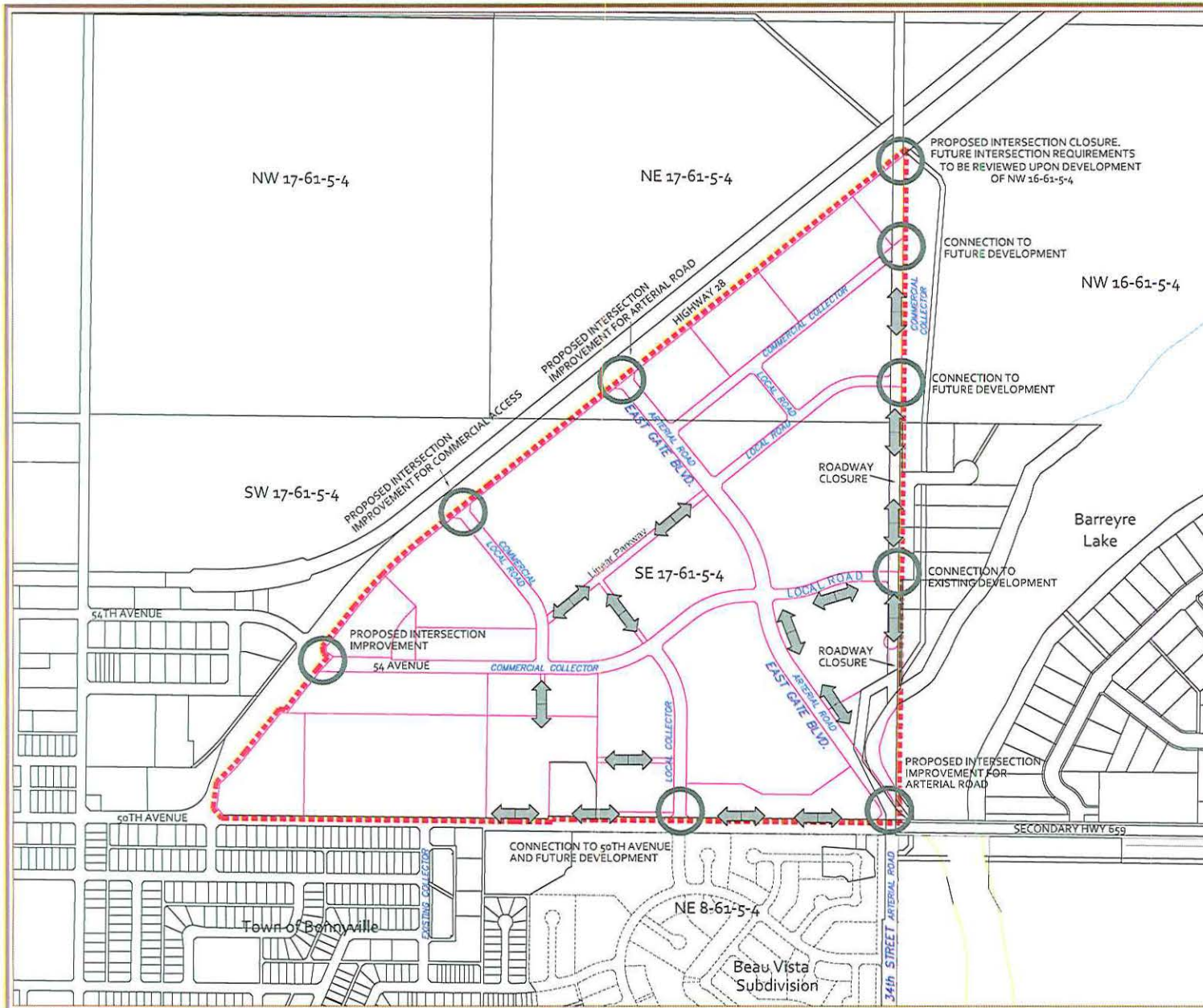
Objectives:

- To provide for the safe and efficient movement of people, goods, and vehicles within the plan area;
- To design and develop a transportation system that takes into account future multi-modal transportation system requirements;
- To improve the appearance of 50th Avenue, Highway 28 and entranceways to Bonnyville; and
- To provide a transportation hierarchy that strives to continue arterial and collector roadways and integrates sound linkages with the existing road pattern, particularly the east boundary road.



MS 53

- ARY



5.3.1.2 Pedestrian Linkages

The plan integrates and connects the entire area to the adjacent communities. Interconnected linear pathways and sidewalks located adjacent to roadways strive to create a walkable community. This aim is achieved using a number of design strategies. A proposed boulevard along the north side of 50th Avenue will include the construction of an undulating berm with appropriate landscaping and a meandering asphalt sidewalk. As stated previously, the intent of the boulevard is to provide added aesthetic value, improve visibility, sense of place, emphasize the importance of the Centennial Centre, and contribute to the enhancement of the 50th Avenue as a major arterial roadway on the east end of Town.

Another key feature of the plan is to ensure well planned and adequate pedestrian linkages are available to commercial, industrial, institutional, and residential uses. Pedestrian sidewalks will be: located adjacent to road ways, interconnected, visible from streets, and tie into key local destinations, such as the Centennial Centre. While linear parkways will generally provide additional walking paths set back from roads within a more natural park setting. Refer to *Figure 9 - Transportation Network*, for an illustration of the pedestrian linear path system.



Illustration II - Concept of 50th Avenue boulevard depicting a sidewalk setback from the street with appropriate landscaping and tree planting to enhance the pedestrian experience.

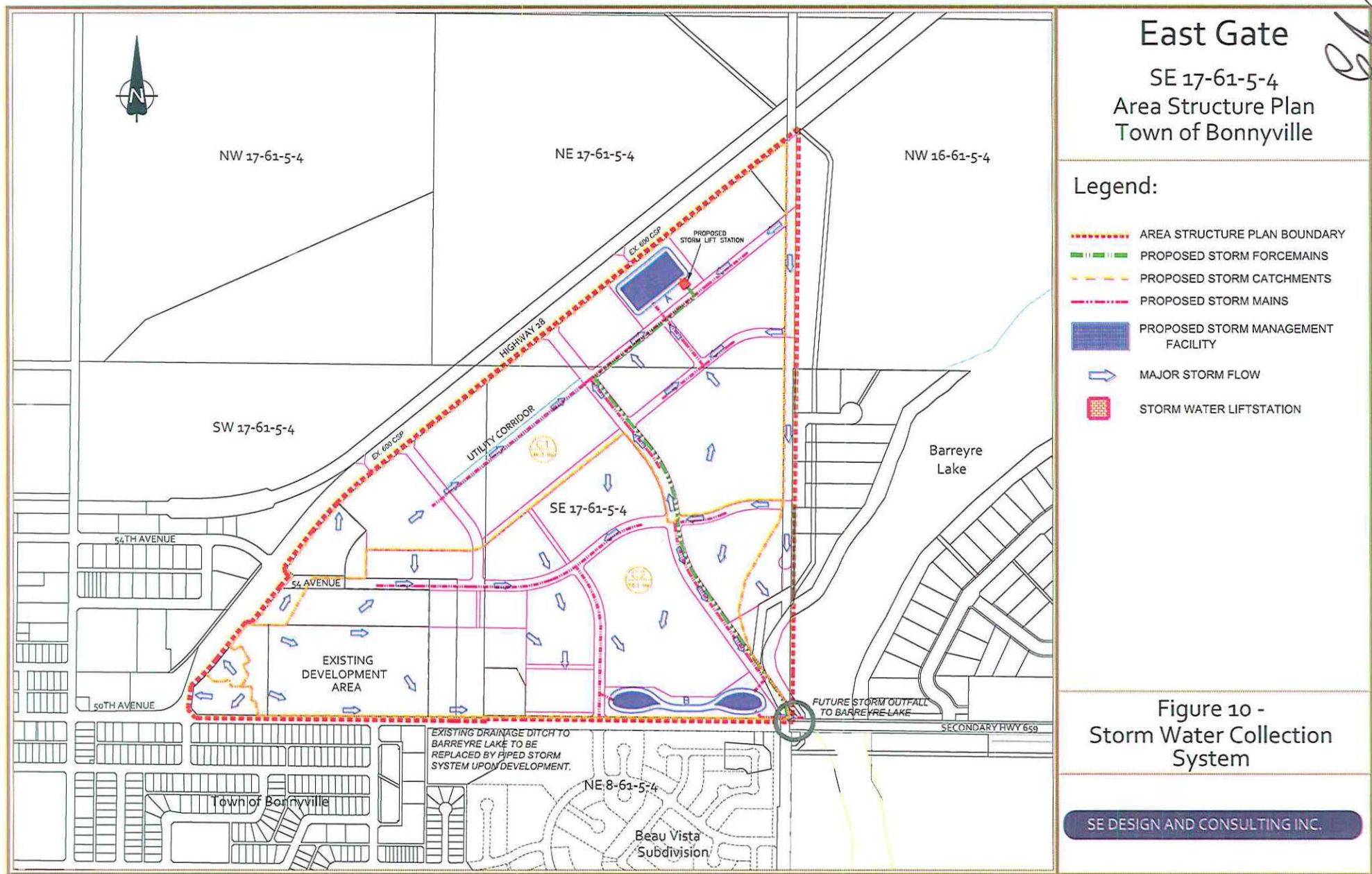
In summary, the town's overall parks and open space system will benefit from new linkages to the existing facilities and features from the developing area. All linear parkways shall be at minimum 10 metres wide and will be in accordance to the Town of Bonnyville 2008 Servicing Standards as amended from time to time. All public open spaces including trails and linear parkways shall be designed according to specifications outlined in Appendix 4 of the MDP and will be landscaped and developed by the applicant to the satisfaction of the Development Authority.

5.3.2 Storm Drainage

The existing site is split into two distinct catchment areas separated by an elevated area in the centre. The north half of the area currently drains into an existing dugout that moves water through a culvert crossing Highway 28 before continuing along a drainage way to an existing wetland located on the quarter section to the northeast. The south portion drains into Barreyre Lake.

The proposed drainage scheme for the proposed development will follow a similar pattern excepting that Pond A flows will be routed to Barreyre Lake as proposed in the Town's Storm Water Master Drainage Plan. The north half of the plan area, Catchment C1 (46.3 Ha), drains toward storm water Pond A and will be discharged through a new storm water lift station and forcemain that will be routed south to a future storm outfall to Barreyre Lake. The development on the south half, Catchment C2 (50.1 Ha), will necessitate the construction of one storm pond and a new storm outfall to Barreyre Lake. The plan proposes utilize one new outfall to Barreyre Lake. The post development storm water flows from each storm pond will be restricted to predevelopment flow rates for the AES 1:100 year storm, as indicated in the Town of Bonnyville Master Drainage Plan. It is intended that both storm ponds will be designed to as wet ponds. Where applicable storm ponds should be located in public open spaces and be subdivided as a Public Utility Lot (PUL) at the high water level mark.

The developer shall be responsible for the construction of the storm water collection system at time of subdivision and all design approvals shall be in accordance with Town of Bonnyville Master Servicing Plan, Master Drainage Plan, Alberta Environment, the Environmental Protection Enhancement Act and best practices at the time of design. The overall layout of the proposed storm water drainage system is shown in *Figure 10 - Storm Water Collection System*.



5.3.3 Sanitary Sewer

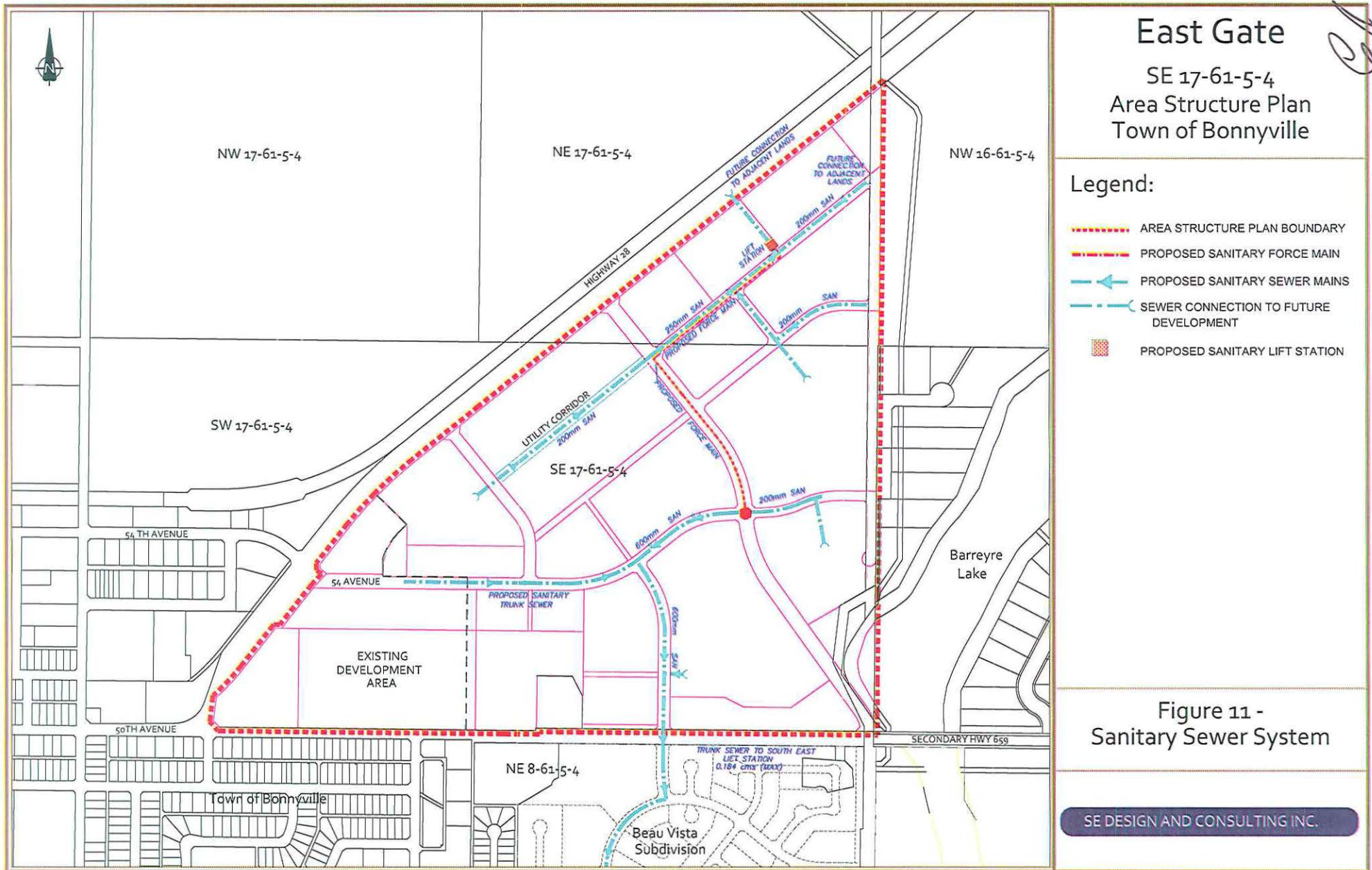
In general, the sanitary system is collected and conveyed through a 600mm sanitary trunk main extension from the Beau Vista development to south of the development area and will be located within road right-of-ways. This 600mm sanitary trunk main ultimately discharges to the Town's south east lift station before being pumped to the sewage lagoon. Within the development area, the main will continue north along the collector road extending north from the Beau Vista plan area to the centre of the East Gate plan area up to the intersection at 54th Avenue before continuing east to the intersection at 34th Street.

Predominantly the development will utilize 200 mm sanitary sewer for local servicing except where sewer capacities dictate an increase in the pipe diameter. The north half of development will require the local sewer mains to be directed to a proposed sanitary lift station within the northeast corner of the development. The effluent will be pumped by the proposed lift station into a sanitary force main that will discharge into the proposed 600mm sewer trunk system.

Future connections of the sanitary sewer system have been provided for the eventual development of both the NE17-61-5-4 and SW16-61-5-4 within the plan. There are no current proposals for development of these lands, but these lands will be able to utilize the additional 'oversized' capacity within the 600mm sanitary trunk main to service these lands. An evaluation of the sewer flows and lift station pump sizing will have to be done at the time of development of these lands to determine available sanitary sewer flows.

The proposed sanitary sewer system is illustrated in *Figure 11 - Sanitary Sewer System*. As development plans progress, at the subdivision application stage the developer should provide information with respect to the design sanitary flow rates in order to confirm the suitability of the proposed tie-in points. The sanitary sewer design will be designed according to the Municipal Servicing Plan and Engineering Design Standards and subject to approval by the Development Authority.





5.3.4 Water Distribution

The development will be fed from three existing main connections outlined in the following manner:

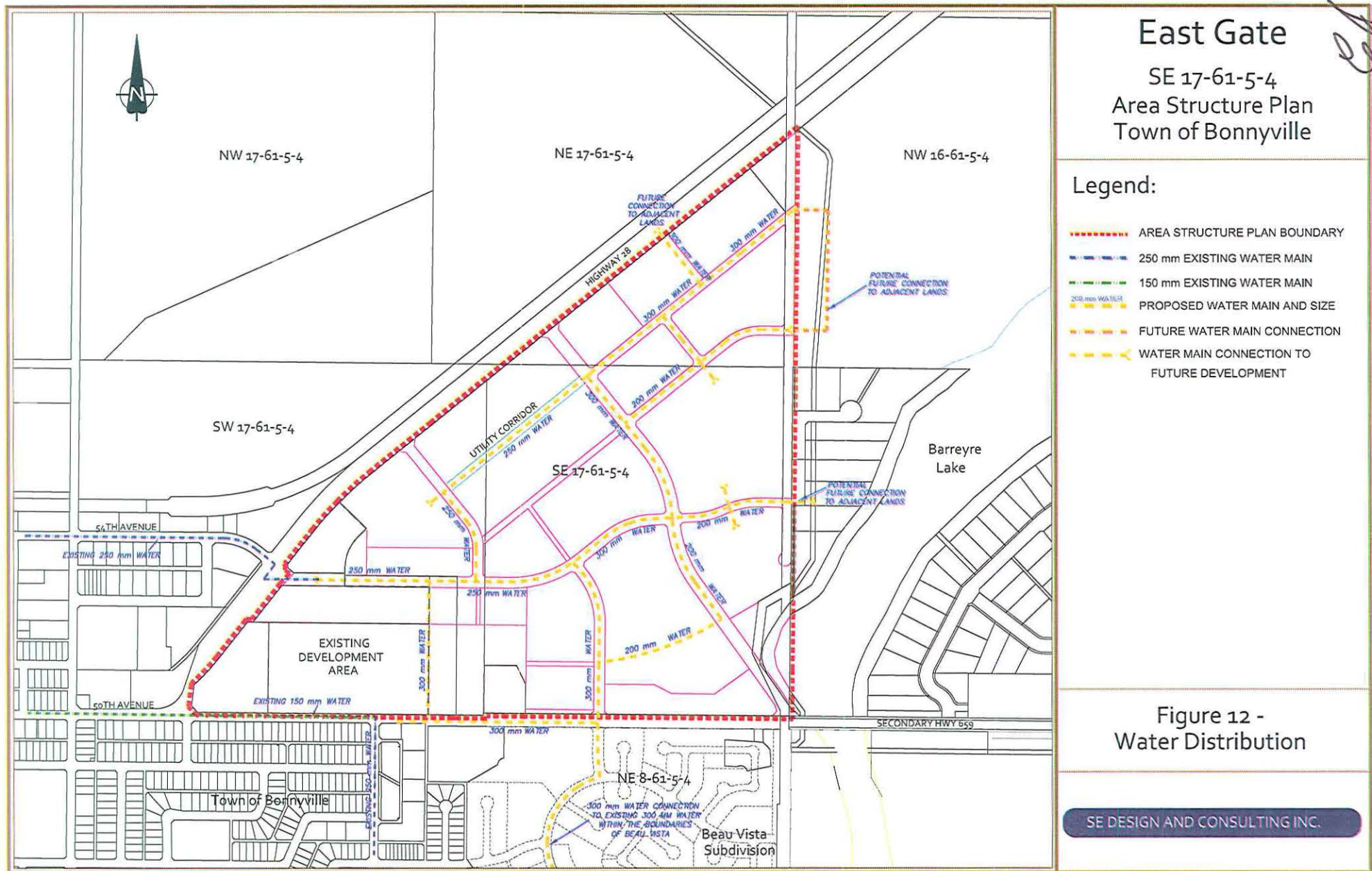
1. 300 mm diameter connection extending north from developing lands to the south, Beau Vista;
2. 250 mm trunk main at 54th Avenue extending from the west to the east boundary; and
3. 250 mm existing connection at 41st Street and 50th Ave extending north.

Predominantly the development will utilize 200mm water mains for localized delivery of water. Larger diameter water mains will be provided within major road ways to provide adequate water supply and pressure throughout the development area. All water distribution system components shall be extended up to the East Gate ASP development boundaries to allow for future developments on the north and east side of the subject area to connect to the Town's system. Provisions include to further extend a 300 mm trunk main line along the arterial road right-of-way going north and other connection to the east for connections to adjacent lands. The design ensures loops are provided for future service areas.

The water distribution system is illustrated in *Figure 12 - Water Distribution System* that clearly identifies the tie-in points, existing piping, proposed piping sizes and dead-end points.

5.3.5 Shallow Utilities

Natural gas, telephone, cable, and power servicing for the proposed development will be installed within the proposed road rights of way and provided for during the subdivision approval process via subdivision registration and upon entrance into a development agreement with the Town of Bonnyville. The proposed shallow utilities will be extended underground from existing utilities adjacent to the property and accommodated for during each phase of the development through Utility Right of Way agreement. The design will be done by each franchised utility company in accordance to both the Town and each shallow utility company requirements.



5.4 Community Services

5.4.1 Emergency and Protective Services

Fire service for the proposed development will be provided from the Bonnyville Regional Fire Authority Fire Station. By-law enforcement and other protective services are provided by the Town and through agreements with local RCMP detachment. Ambulance service will be provided from the Town of Bonnyville.

5.4.2 Schools

Based on the information provided by the relevant school authorities, public schools for students of this area are as follows:

Secondary Schools

Centralized High School 4908 49 Avenue
Ecole Notre Dame High 5006 49 Avenue

Primary Schools

Duclos School 4801 52 Avenue
Ecole Dr. Bernard Brosseau School 4301 - 38 Street
H.E. Bourgoin Middle School 4313 39th Street
Notre Dame Elementary School 4711- 48th Street

Francophone schools

Ecole Des Beaux-Lacs 4312 34th Street

Northern Lights School Division (NLSD) was the only school authority to indicate a potential need for a site in the development area. The NLSD is requesting a 15 acre parcel in close proximity to the Centennial Centre in the East Gate development.

The ASP provides two alternative schemes as options for the school reserve site. These two options are illustrated in Figures 8 and Figure 8A. Figure 8 illustrates the ASP recommended site because of its close proximity to the Centennial Centre. The Figure 8 option provides 3.55 hectares of usable space including the school building. As suggested by the school authority the Centennial Centre "was designed to accommodate students and their related activities".



The potential exists, not only to accommodate curriculum activities but also specialized programs such as hockey and/or soccer schools.” Any additional space requirements could be overcome through joint use agreements between the Town and the school authority for the adjacent public lands.

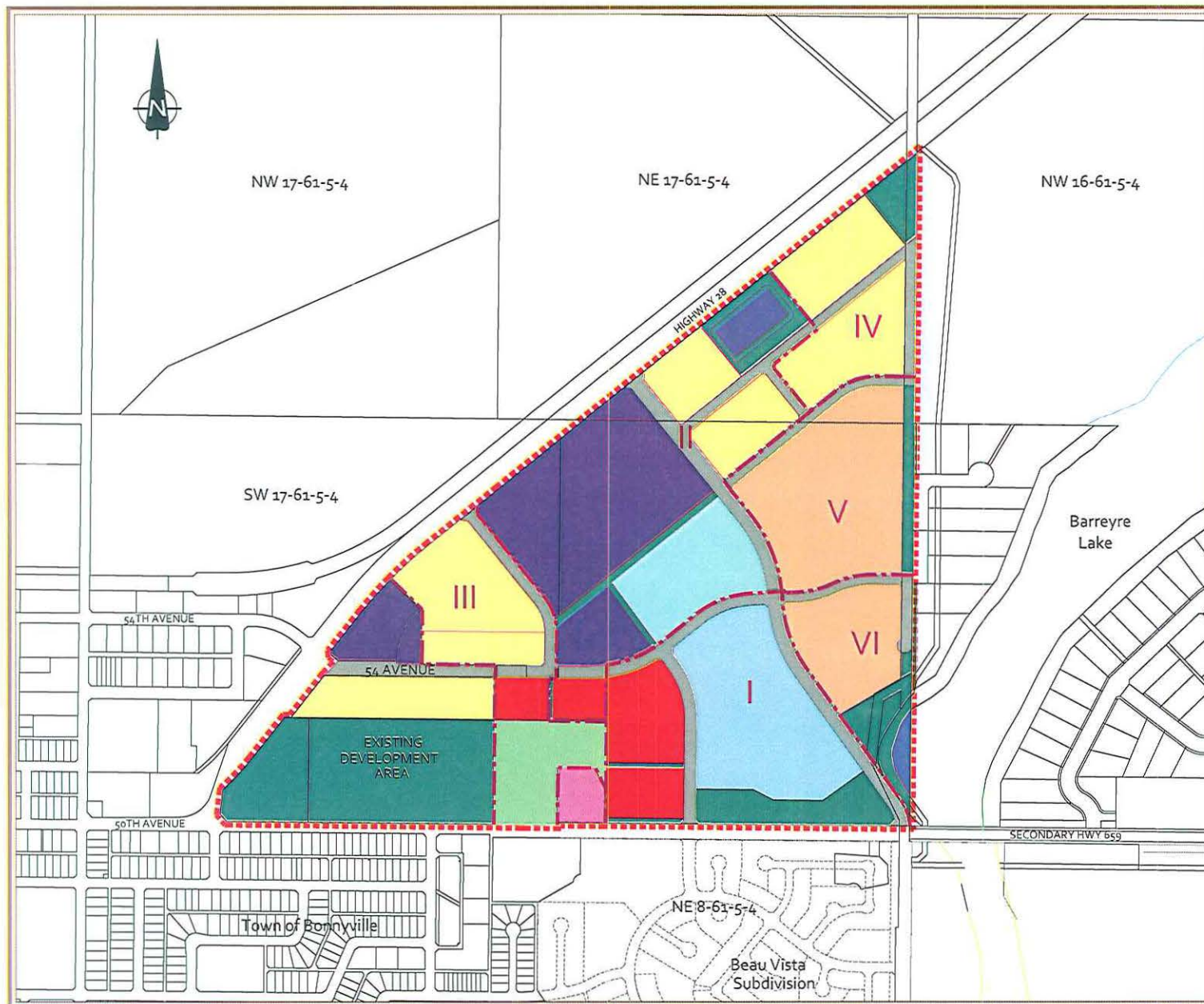
6 DEVELOPMENT SEQUENCE

Phases will be based on market demands for specific land uses and the ability to connect to municipal water and sanitary services. It is anticipated that building in the plan area will generally occur contiguously with existing and newly constructed development. ‘Leap frogging’ of undeveloped lands should generally be avoided to extend municipal services and expanding the local transportation network in a cost effective manner.

It is anticipated that the development will initially focus on the lands designated for mixed residential-commercial uses and the residential manufactured home park along the south portion of the plan area. After completion of the first phase, future development will be directed toward the shopping centre commercial, then highway commercial uses and finally concluding with lower density residential land use areas. Future development phasing will take into account the need to provide looping water systems, temporary and permanent vehicle access and the logical extension of municipal services. The implementation of each phase will begin with the preparation of a detailed outline plan and a legal plan of subdivision in response to market conditions. Refer to *Figure 13 - Phasing Strategy* for an illustration of the anticipated development staging.



SE Design



East Gate

SE 17-61-5-4
Area Structure Plan
Town of Bonnyville

Legend:

- AREA STRUCTURE PLAN BOUNDARY
- SHOPPING CENTER COMMERCIAL
- RESIDENTIAL MANUFACTURED HOME
- SINGLE FAMILY RESIDENTIAL
- HIGHWAY COMMERCIAL
- MULTIFAMILY RESIDENTIAL
- INSTITUTIONAL
- PUBLIC OPEN SPACE
- ENVIRONMENTAL RESERVE
- MIXED USE COMMERCIAL RESIDENTIAL
- STORM MANAGEMENT PONDS
- PHASE BOUNDARY

Figure 13 -
Phasing Strategy

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APPENDICIES/ATTACHMENTS

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July 8, 2010

Project File: 4835-09-165

Ms. Karolina Drabik
SE Design and Consulting Inc.
Suite 1005 - 5101 46th Avenue
Cold Lake, AB
T9M 0C8

Dear Ms. Drabik:

**SUBJECT: TOWN OF BONNYVILLE
EASTGATE AREA STRUCTURE PLAN
PTN. NE, SE & SW 17-61-5-W4M
HISTORICAL RESOURCES ACT REQUIREMENTS**

Thank you for providing an information package for the above-cited project. I apologize for the delay in our response. Ministry staff have reviewed the potential for the proposed project to have an impact on intact historic resources and have concluded that, due to the amount of previous land disturbance, an Historic Resources Impact Assessment is not required. Therefore *Historical Resources Act* clearance is granted for this project as described in the information package.

HISTORICAL RESOURCES ACT REQUIREMENTS

Reporting the discovery of historic resources: Pursuant to Section 31 of the *Historical Resources Act*, should any archaeological resources, palaeontological resources, Aboriginal traditional use sites, and/or historic period sites be encountered during land disturbance activities, the Historic Resources Management Branch must be contacted immediately. It may then be necessary for further instructions regarding the management of these resources to be issued.

Should you require additional information or have any questions concerning the above, please contact me at 780-431-2301 or by e-mailing john.brandon@gov.ab.ca.

On behalf of the Historic Resources Management Branch, I would like to thank you for your cooperation in our endeavour to conserve Alberta's past.

Sincerely,



John Brandon
Head, Land Use Planning



Freedom To Create, Spirit To Achieve.

A handwritten signature in blue ink, likely belonging to a member of the Historic Resources Management Branch.

MUNICIPAL RESERVE AREA CALCULATIONS - EASTGATE DEVELOPMENT (BONNYVILLE, AB)

Parcel Number	Owner	Legal	Area of Plan (Ha)	MR Owing (Unsubdivided state)	MR Owing (DRC)	Comments	Total (Ha.)
1	Envision Ventures Inc.	092 307 374	5.61	0.56	0.77		1.33
2	Envision Ventures Inc.	092 419 379 +1	2.02	0.20			0.20
3	Envision Ventures Inc.	092 419 379	6.95	0.69			0.69
4	Western Canadian Alliance	912 094 196	0.99	0.00		DRC	0.00
5	Envision Ventures Inc.	092 419 029	52.63	5.26	0.10		5.36
6	Envision Ventures Inc.	042 484 681	16.30	1.63			1.63
7	JAM Projects Inc.	072 354 277	1.56	0.16			0.00
8	NLC Equipment	072 218 531+2	3.12	0.31			0.00
9	Town of Bonnyville	211Q186	7.59	0.76	0.26		0.00
10	Town of Bonnyville	812 047 499	2.60	0.00			0.00

TOTALS	99.38	9.58	1.13		9.22
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ASP Development Area 103.02

Development Area (Envision Ventures) 83.52

MR Required for Development Area 8.35

MR Required for Deferred Reserve 1.13

Total MR Required for Development Area 9.48

MR Provided for Development Area 8.66

MR To Be Provide Through Outline Plans 0.82

Addition Public Land Dedicated 1.63

Existing Park and Open Space 10.19

Total Park and Open Space Within ASP Area 21.30